

**DRAFT**

## **Division III**

### **Residential Sub Area Standards**



## Residential Sub Area Standards

Unless otherwise noted in this document, all development in the residential sub areas shall be consistent with the applicable residential districts development standards in the LBMC, as shown in Table 1.

### Permitted Uses

#### *Residential Uses*

Consistent with the Sub Area Map (Figure 4 ), the following uses shall apply to those sub areas.

**Table 2 : Residential Use Table**

Use	Sub Area 1A and 1B	Sub Area 2	Sub Areas 3 and 6	Sub Area 4	Sub Area 5
Single-family detached or attached	N	Y	Y	Y	Y
Multi-family dwelling	Y	N	Y	N	Y
Townhouse	Y	Y	Y	N	Y
Secondary housing units (see Section 21.51.275)	N	N	N	A	N
<b>Other Uses</b>					
Child day-care home - small or large facility [1-14 persons] (see Sections 21.52.249 + 21.15.480 + 21.15.490)	N	N	N	A	N
Electrical distribution station (see Section 21.52.223 + 21.15.960)	N	N	Y (Through SPR only in Sub Area 3)	N	N
Group home (1-6 persons) (see Section 21.15.1200)	Y	Y	Y	Y	Y
Existing Building 1C - Boeing Data Center	Y	N	N	N	N

Y = Yes      N = No      A = Accessory Use

Those uses which are permitted subject to an Administrative Use Permit or Conditional Use Permit shall be subject to the same processes defined in the LBMC.

#### *Commercial uses in Mixed Use Overlay Zone*

Sites fronting on both the northerly and southerly edges of “F” Street between Lakewood Boulevard and 2<sup>nd</sup> Avenue shall permit the commercial uses of the Neighborhood Commercial, Pedestrian-oriented (CNP) zoning district, Chapter 21.32 of the LBMC.

### Permitted Density

Table 3 establishes the permitted densities in each sub area. In calculating the number of units permitted on a site, the following shall apply:

#### *Maximum Number of Dwelling Units*

The maximum number of dwelling units in all residential sub areas shall not exceed 1,400 units. Note that using the Maximum Density (Dwelling Units per acre) throughout each sub area may yield an arithmetic result greater than 1,400 units, and therefore the Maximum Density is only intended to provide limited flexibility in the location of density, and shall not be interpreted as an entitlement.

**Table 3 : Permitted Lot Size & Density in Residential Sub Areas**

Sub Area	No. of Units / Lot	Minimum Lot Area (including alley easements)	Minimum Lot Width (excluding alley easements)	Maximum Density
Sub Area 1A	N / A	18,000 sq.ft.	120 ft.	50 DU / acre
Sub Area 1B	N / A	18,000 sq.ft.	120 ft.	50 DU / acre
Sub Area 2	N / A	18,000 sq.ft.	22 ft. *	20 DU / acre
Sub Area 3	N / A	18,000 sq.ft.	120 ft.	25 DU / acre
Sub Area 4	One	3,500 sq.ft.	35 ft.	14 DU / acre
		4,500 sq.ft.	45 ft.	10 DU / acre
Sub Area 5	N / A	18,000 sq.ft.	120 ft.	27 DU / acre
Sub Area 6	N / A	18,000 sq.ft.	120 ft.	25 DU / acre

**The maximum number of dwelling units in all residential sub areas combined shall not exceed 1,400 units**

**There will be a minimum of 100 lots of at least 4,500 s.f. in Sub Area 4**

*\* Refers to Unit Width - not Lot Width*

*Net Lot Area*

All densities are calculated on the basis of net lot area (lot size minus any required dedications for public right-of-way improvements)

*Fractional Densities*

Fractional densities shall not be counted in determining the number of units allowed on the site.

**Setbacks**

Setbacks shall be provided for the purpose of providing light, air, pedestrian and vehicular circulation, emergency access and general aesthetic improvements. All lots shall have one front yard and one rear yard, with the exception of through lots, which may have two (2) front yards and no rear yard. All other property lines for all lots shall be considered side property lines.

All setbacks shall be provided as specified in the Special Development Standards section (Division III) of this document. Semi-subterranean and subterranean parking garages must comply with the required building setbacks.

*Permitted projections*

The projections permitted into the required setbacks shall be those permitted in Table 31-3 of LBMC Section 21.31.215.

*Corner Cut-off*

At corners of properties at street intersections, intersections of streets and alleys and at intersections of driveways and property lines, new construction shall not be permitted in such a manner that it significantly impedes visibility between heights of 3 feet and 7 feet above the ground in a corner cut-off of at least 6 feet by 6 feet (measured from the intersection of the property lines or the intersection of a driveway, alley, and a property line). One vertical support element such as a column is permitted in the corner area, if the cross-section of the element fits within a circle with a diameter of 18 inches. This requirement may be waived through the Site Plan Review process if the Director of Public Works finds that the cut-off is not necessary

### Usable Open Space

Usable open space in terms of square feet per dwelling unit shall be provided as indicated in Table 4 of this document, unless noted otherwise. In all sub areas except Sub Area 2 and 4, each dwelling unit shall provide fifty percent (50%) of the open space as common open space and fifty percent (50%) as private open space. See Special Development Standards for each sub area for additional requirements and/or exemptions. Actual percentages can be adjusted through Site Plan Review.

The design of the open space shall comply with the open space requirements of LBMC Section 21.31.230.

**Table 4 : Usable Open Space Requirements in Residential Sub Areas**

<b>Sub Area</b>	<b>Min. Usable Open Space per Unit</b>
<b>Sub Area 1A</b>	150 sq. ft. *
<b>Sub Area 1B</b>	150 sq. ft. *
<b>Sub Area 2</b>	250 sq. ft. **
<b>Sub Area 3</b>	150 sq. ft.
<b>Sub Area 4</b>	10% of Lot Area ***
<b>Sub Area 5</b>	150 sq. ft.
<b>Sub Area 6</b>	150 sq. ft.

\* Private open space requirements for units facing Lakewood Boulevard or "F" Street can be met using common open space.

Refer to the special development standards for additional usable open space requirements for the mixed-use overlay zone.

\*\* 100 percent private open space is allowed.

\*\*\* Minimum usable open space dimension is 8 ft. Front porches within front setbacks with a minimum depth of 8 ft. can be used as part of the required open space.

All open space requirements must be accomplished within individual parcels. However, on lots less than 4,500 sf, off-set side yard easements may be allowed at the discretion of the Director of Building and Planning, but area not on the subject parcel may not be used to meet minimum dimension or area requirements.

At least 50% of the minimum usable open space must be open to the sky.

Definitions of Usable Open Space, Common Open Space and Private Open Space shall be per LBMC.

**Minimum Courtyard Dimensions**

In multi-family projects, and in the event that courtyards are used as part of the Usable Open Space requirement, courtyards shall be designed to create an attractive and comfortable on-site open space area in order to enable residents to enjoy outdoor living and recreational activities. Courtyard designs shall conform to the standards as specified in LBMC Section 21.31.242.

**Residential Amenities**

Multi- family residential buildings in Sub Areas 1, 3, 5 and 6 consisting of 20 or more units shall provide a common recreation/ multi-purpose room or rooms of three hundred (300) or more square feet furnished with recreational facilities, or other recreational amenities such a swimming pool, play equipment, or other facilities directed to a specific demographic section of the housing market which may be deemed to be of equivalent value by the Site Plan Review Committee or Planning Commission.

Additionally, residential developments shall provide storage cabinets or storage rooms and/or bicycle storage areas to the satisfaction of the Site Plan Review Committee.

**Privacy Standards**

Privacy standards shall apply to all multi-family residential development, and shall be consistent with LBMC Section 21.31.240, unless waived through the Site Plan Review Process.

**Accessory Structures**

With the exception of setbacks, which are set by this document; attached and detached accessory structures shall be subject to the development standards specified in LBMC Section 21.31.245.

**Pedestrian Access**

Location and design of building entries and elevators in the case of multi family residential development shall be subject to the development standards specified in LBMC Section 21.31.250.

**Distance Between Buildings**

Minimum separation of two (2) or more detached principal use buildings on the same lot shall be eight feet (8').

### **Mechanical Equipment Screening on Rooftops**

All mechanical equipment on rooftops shall be screened from public view by solid screening devices at least as high as the equipment being screened. Development standards specified in LBMC Section 21.31.265 shall apply.

### **Trash Receptacles**

All trash areas shall be screened from public view on all sides. See LBMC Section 21.46.080 (21.31.245?) (Accessory Structures) for additional requirements.

### **Utility Meters Screening**

All utility meters shall be fully screened from view from a public right-of-way. The development standards in LBMC Section 21.31.270 shall apply.

### **Undergrounding of Utilities**

All projects requiring site plan review shall underground all overhead utility service to the site. The utility company's design of the electric system, including locations and aesthetic treatment, shall be in accordance with the regulations of the Public Utilities Commission of the State of California.

### **Off-street Parking and Loading Requirements**

Parking and loading areas shall be provided as required in LBMC Chapter 21.41 (Off-Street Parking and Loading Requirements).

### ***Guest Parking Requirement***

The required parking for guests may be fulfilled by those parking spaces provided on-site as well those curbside parking spaces along the frontage of the streets immediately adjacent to the residential use. Street parking on both sides of "F" Street between Lakewood Boulevard and Second Avenue shall not be used to satisfy this requirement.

### **Vehicular Access**

Curb cuts are prohibited along Lakewood Boulevard and Carson Street with the exception of one right-in / right-out driveway for Sub Area 3.

### **Landscaping Requirements**

All lots in residential districts shall be landscaped as provided for in LBMC Chapter 21.42 (Landscaping Standards). The following exceptions shall apply:



*Street Trees*

- Street frontage within Sub Areas 1, 2, 3, and 5. Within the required setback area along all street frontages, except driveways, a minimum five-foot-wide (5') landscaping strip (inside dimensions to planter) shall be provided. This area shall be landscaped with one (1) tree for each twenty (20) linear feet of street frontage. These trees may be grouped, but at least one (1) group shall be located within each 100 feet of street frontage. Accent trees shall be planted at areas of significance, such as, intersections, pedestrian paseos, and key site heritage locations. For each tree three (3) shrubs shall be provided. This supersedes LBMC Section 21.42.040 B1 (Landscaping standards-R-3, R-4, and non residential districts excluding IM, IG, and IP industrial districts).
- For required trees, at least twenty-four inch (24") box (but not less than seven feet (7') in height) shall be provided. Accent trees that establish a point of significance, as described above, shall be thirty-six inch (36") box (but not less than ten feet (10') in height). This supersedes LBMC Section 21.42.040 C1 (Landscaping standards-R-3, R-4, and non residential districts excluding IM, IG, and IP industrial districts).
- Street trees shall be spaced as provided for in LBMC Section 14.28.020 (Planting). Alternate tree spacing may be approved by the Director of Public Works in accordance with the LBMC, these Standards, and the Master Street Tree Plan (See Division V).

*Parkway Landscaping*

- Plant material in parkways may be turf, groundcover, flowers, shrubs, and street trees. This supersedes LBMC Section 21.42.060 C2 (Landscaping standards-Public right-of-way).
- All new developments shall be required to provide street trees in the abutting parkway/sidewalk areas. The suggested species of street trees and spacing are listed in Division V. Alternative species may be approved through the Site Plan Review process and may be appealed to the Director of Planning and Building and the City Planning Commission.

**Fences and Garden Walls**

Fences and garden walls are permitted accessory structures subject to the development standards contained in LBMC Chapter 21.43 (Fences and Garden Walls).

**Signs**

On-site signs are permitted accessory structures subject to the development standards contained in LBMC Chapter 21.44 (On-Premises Signs).

## Special Development Standards

### Sub Area 1A:

Located along Lakewood Boulevard, Sub Area 1A is intended to create an articulated and consistent urban edge of multi-story residential buildings that relate to the scale of the boulevard, which shall act as buffers for the lower density residential uses to the west. A small public park at the corner of Lakewood Blvd. and Carson Street is adjacent to this sub area and shall be taken into consideration in the design of this sub area.

#### Setbacks and Building Stepbacks

**Table 5 : Setbacks for Residential Sub Area 1A**

Minimum Street Setback	Lakewood Blvd. Setback	Minimum Bldg. to Bldg. Setback	Minimum Alley Setback
15'*	26'	8'	15' **

\* Stoops may encroach into the Street Setback

\*\* Measured from property line at centerline of alley. Minimum dimension from edge of alley is 5 ft.

Building stepbacks are in addition to building setbacks, and are established to create height and bulk transitions between buildings and public streets/ alleys/ parks, as well as between higher density uses and lower density uses. These transitions shall be controlled by building stepback requirements as shown in the illustrative sections of this document. Where stepbacks are required, 25% of the frontage is allowed without a stepback.

#### Build-to line standard

For build-to- lines in Sub Area 1A, at least 60% of the ground floor building frontage at a minimum height of 15' shall be constructed along the established build-to-line. Articulated courtyard walls built along such line can be used to satisfy this requirement.

**Facade Articulation**

No continuous building wall shall extend more than 60 feet in width without a facade articulation element. Facade articulation shall consist of elements such as expressed structural bays, pilasters, moldings, recessed wall panels, or display features to create visual interest.

**Blank Walls**

Blank walls are not allowed along build-to-line frontages. Along other frontages, the maximum length of a blank wall without articulation or relief of at least 6 inches in depth shall be 25 feet.

**Private Usable Open Space/ Balconies**

Balconies along the Lakewood Boulevard edge are discouraged. The requirement for private usable open space for units affected by this provision may be waived through Site Plan Review, provided that equivalent common usable open space is provided on site.

**Vehicular Driveway Access**

Vehicular driveway access shall be taken via the alleys serving the site, or secondary neighborhood streets that will not disrupt pedestrian circulation. Access streets and driveways are encouraged to reinforce the grid imposed by the street hierarchy of the Plan Area. Mid-block driveway access along Lakewood Boulevard is not allowed. These requirements may be modified through the Site Plan Review process.

**On-Grade Parking Garages***Location*

On-grade garages must be screened on street sides by residential units, the building entrance lobby or other portions of the building. The screening requirement may be waived through the Site Plan Review process only if the garage wall is deemed to be architecturally integrated into the design of the overall building, and is located along a secondary street.

*Architectural treatment*

Parking structures must be designed with the same care and attention as the buildings they serve. They shall be compatible in architectural treatment and detail.

*Screening*

Ramps, cars, and sources of artificial lighting in parking structures shall be minimally visible from public streets, public parks, and residential uses.

## Sub Area 1B:

Located along “F” Street, this sub area shall consist of a Mixed Use Overlay zone, which shall contain a mix of multifamily residential uses, with active ground floor retail storefronts and pedestrian serving uses with special on-street parking provisions.

**Table 6 : Setbacks for Residential Sub Area 1B**

Minimum Street Setback	“F” Street Setback	Lakewood Blvd. Setback	Minimum Bldg. to Bldg. Setback
15’	2’	26’	8’

\* *Private open space requirement for units facing Lakewood Blvd or ‘F’ Street can be met using common open space.*

### Setbacks and Building Stepbacks

Building stepbacks are in addition to building setbacks, and are established to create height and bulk transitions between buildings and public streets/ alleys/ parks, as well as between higher density uses and lower density uses. These transitions shall be controlled by building setback/ stepback requirements as shown in the illustrative sections included in this section of the document.

For four-story buildings, 70% of the “F” Street-fronting facade must provide an eight-foot step-back. The step-back may occur above either the second or third floor. The remaining 30% of the street-fronting facade may be constructed with no additional setback or step-back requirements. For three-story buildings, 30% of the “F” Street-fronting facade must contain an eight foot minimum step-back. The step-back may occur above either the first or second floor.

This criteria may be waived during Site Plan Review at the discretion of the Director of Planning and Building or the Planning Commission if the height of the street wall proposed is consistent with the existing or proposed street wall on the opposite side of the street.

**Mixed Use Overlay Zone**

Frontages along “F” Street shall provide pedestrian-oriented uses, which are generally consistent with the CNP zone in Chapter 21.32 of the LBMC and will include the following:

- Restaurants & ready-to-eat foods
- Retail sales
- Personal service uses
- Lobbies of: hotels, office buildings, residential developments, movie/live theaters, or of any other entertainment uses
- Public plazas and outdoor dining areas.

Pedestrian-oriented uses shall occupy at least 60% of the ground floor building frontage on streets where active pedestrian uses are required.

***Minimum First Floor Height***

The minimum first floor height of uses on the ground floor shall be 16 feet above the adjacent sidewalk. Exceptions to this requirement may be granted through Site Plan Review.

***Display/Clear Window Requirement***

Clear, non-reflective display windows/doors shall comprise at least 60% of the ground floor street facade of pedestrian-oriented uses. The maximum height of the bottom sill of required display windows shall not exceed 30 inches above the adjacent sidewalk. The minimum head height for storefronts and windows at the ground floor should be 10 feet above the adjacent sidewalk.

***First Floor Elevation***

The first level of buildings which require ground floor pedestrian oriented uses shall have a floor elevation which approximates the elevation of the adjacent sidewalk.

***Setback***

Required setback along streets with pedestrian serving uses shall be hardscape and shall be considered an extension of the sidewalk. No landscape of such setback is allowed.

*Awnings and Canopies*

Store front canopies and/ or awnings are required unless waived through the Site Plan Review process. The minimum vertical clearance between the ground or street level and the bottom of the awning should be 10 feet. Awnings should be placed below the ground floor cornice (or below the sills of the second story windows if no cornice exists). Awnings should be divided into sections to reflect the major vertical divisions of the facade. The awning/canopy may encroach over the public sidewalk provided at least 4 feet of clearance is maintained from the street curb line. For awnings and canopies, the materials, shape, rigidity, reflectance, color, lighting, and signage, should relate to the architectural design of the building.

*Entrances Facing the Street*

Entrances to uses on ground and upper floors must open onto the public right-of-way. Entrance doors should be setback at least three feet from the property line in order to avoid encroachment on to right-of-way.

**Build-to-line standard**

For build-to- lines along “F” Street, at least 60% of the ground floor building frontage at a minimum height of 15’ shall be constructed along the established build-to-line. Arcades, colonnades, porches, and articulated courtyard walls built along such line can be used to satisfy this requirement.

If the remaining portion of the ground floor is setback, it shall not be more than 20’ in length nor setback more than 10’ from the build-to-line, in order to maintain the continuity of the intended street wall.

**Facade Articulation**

No continuous building wall shall extend more than 60 feet in width without a facade articulation element.

Blank walls are discouraged, and the maximum width of a blank wall without articulation or relief of at least 6 inches in depth shall be 25 feet. Facade articulation shall consist of elements such as expressed structural bays, pilasters, moldings, recessed wall panels, or display features to create visual interest.

**Building Height**

Additional building height on the first 100 feet of building depth fronting “F” Street (measured from “F Street property line) may be allowed through the Site Plan Review Process provided that such additional height is not used to increase the number of habitable floors allowed in this document, and the additional height is used for architectural features that are consistent with the “main street” vision for the street. In no case shall the allowed increase in height be more than 10 feet above the allowed building height.

**Private Usable Open Space/ Balconies**

Balconies for the first two floors of “F” are not allowed, unless waived through Site Plan Review. The requirement for private usable open space for units affected by this provision may be waived through Site Plan Review, provided that equivalent common open space is provided on the same parcel.

**Vehicular Driveway Access**

Vehicular driveway access is prohibited along frontages which require pedestrian-oriented uses. All other vehicular access shall be taken via the alleys serving the site or secondary neighborhood streets that will not disrupt pedestrian circulation. Access streets and driveways are encouraged to reinforce the grid imposed by the street hierarchy of the Plan Area. These requirements may be modified through the Site Plan Review process. Mid-block driveway access along Lakewood Boulevard is not allowed.

**On-Grade Parking Garages***Location*

On-grade garages must be screened on street sides by residential units, the building entrance lobby or other portions of the building. The screening requirement may be waived through the Site Plan Review process only if the garage wall is deemed to be architecturally integrated into the design of the overall building, and is located along a secondary street.

*Architectural treatment*

Parking structures must be designed with the same care and attention as the buildings they serve. They should be compatible in architectural treatment and detail.

*Screening*

Ramps, cars, and sources of artificial lighting in parking structures shall be minimally visible from public streets, public parks, and residential uses.

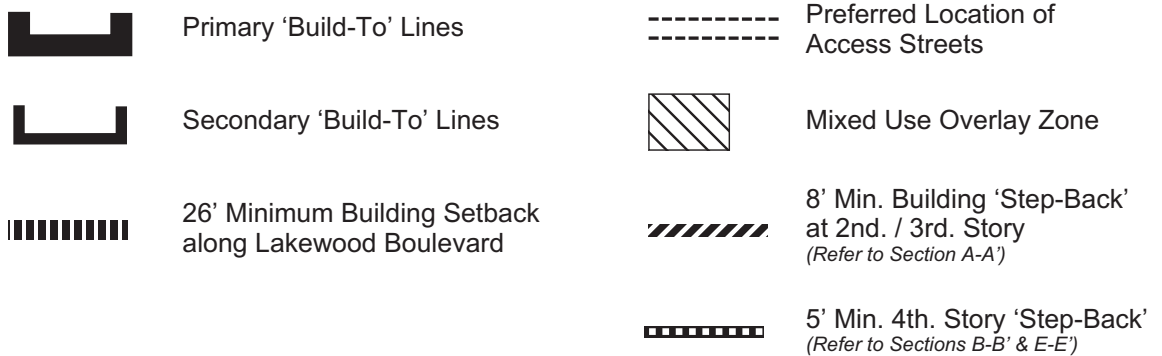
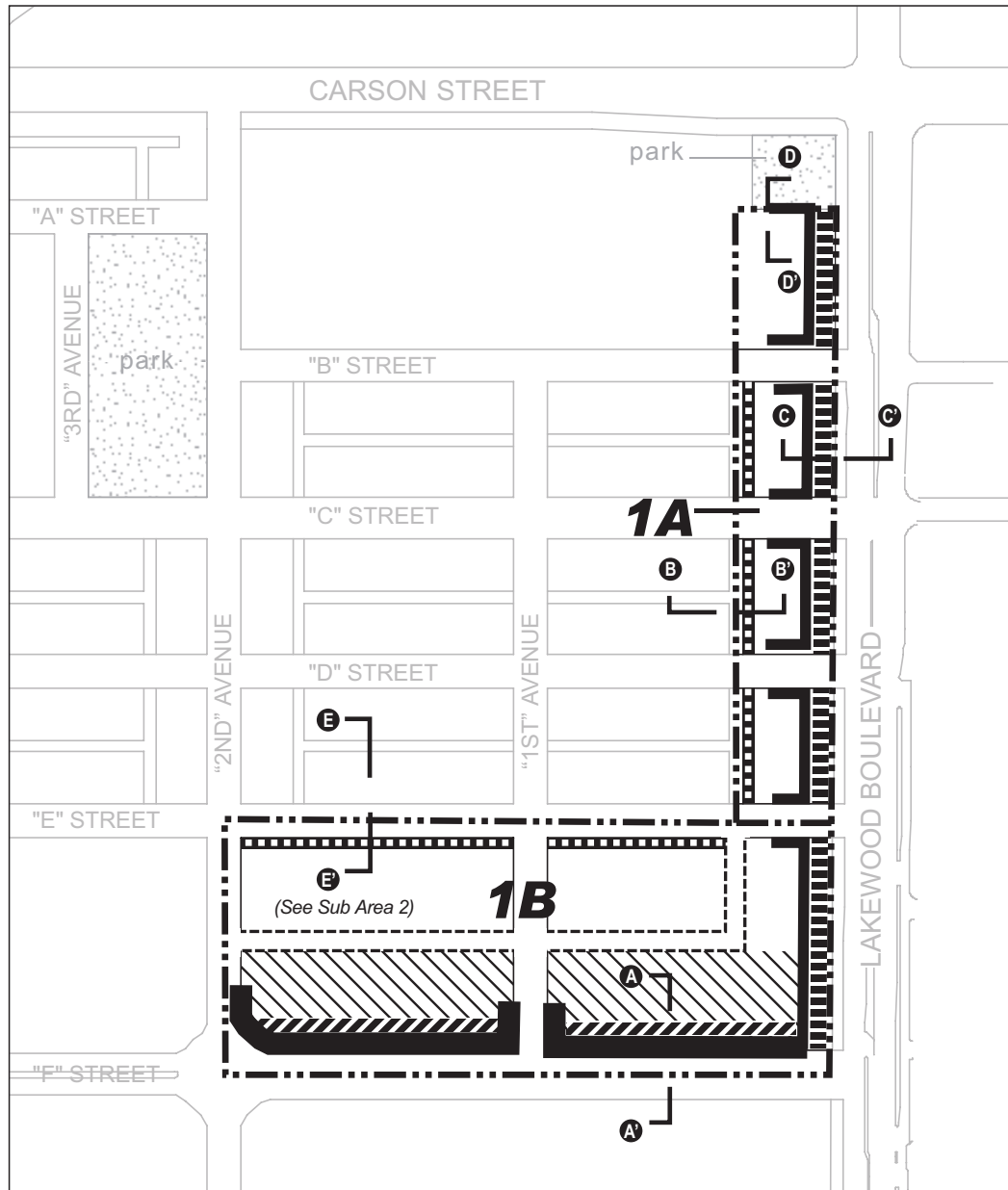
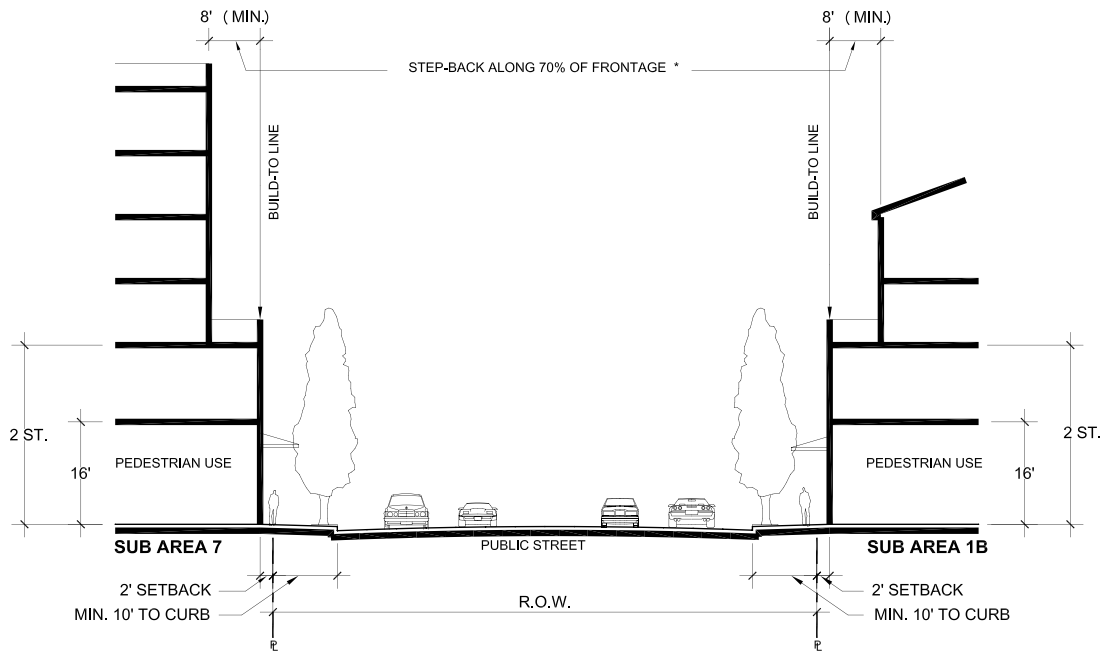


Figure 11 : Sub Areas 1A and 1B





\*NOTE: REFER TO STEP-BACK REQUIREMENTS FOR ADDITIONAL INFORMATION

Figure 12 : Section A-A'

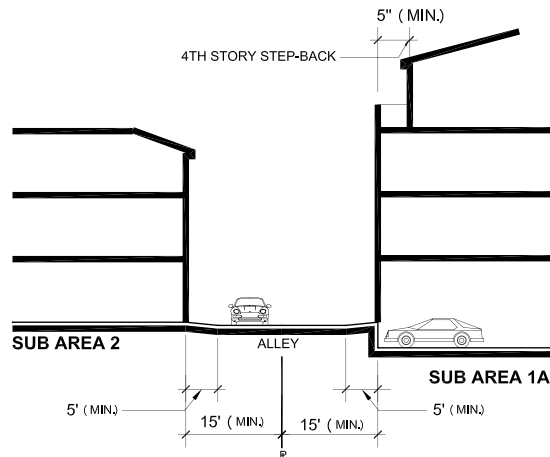


Figure 13 : Section B-B'

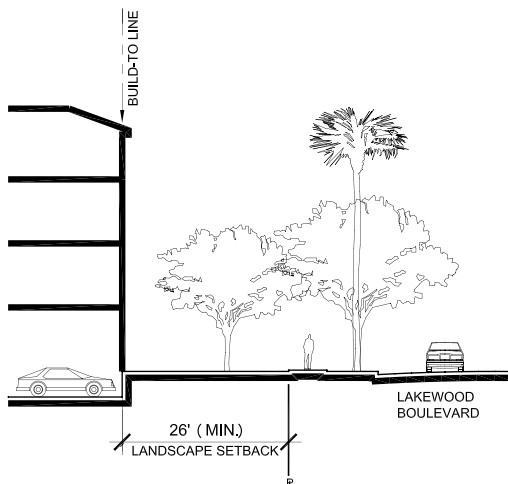


Figure 14 : Section C-C'

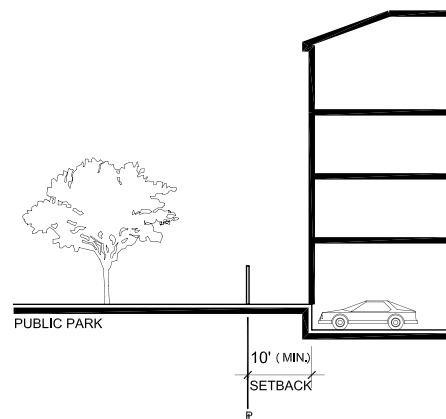


Figure 15 : Section D-D'

## Sub Area 2:

This sub area shall consist of townhouse (traditional row house) residential buildings. It is intended to become a major contributor to the character of the planned area with consistent building edges, lush landscaped streets, and use of private alleys for garage access. This sub area will become the transition between higher density residential uses along Lakewood Boulevard, and the lower density residential uses to the west. “C” Street leading to the park between 2<sup>nd</sup> and 3<sup>rd</sup> Avenues is envisioned as the main residential entry way to the neighborhood.

### Setbacks and Building Stepbacks

**Table 7 : Setbacks for Residential Sub Area 2**

Minimum Street Setback	Minimum Rear Setback	Minimum Alley Setback	Minimum Bldg. to Bldg. Setback
10' *	15' **	15' @ alley ***	10'

\* Stoops and Porches may encroach 5 ft. into the Street Setback.  
Porches shall be as defined in the LBMC.  
Street setback to a 3rd story is 15 ft.

\*\* Measured from property line at centerline of alley.  
Rear setback to a 3rd story is 20 ft.

\*\*\* Measured from property line at centerline of alley.  
Minimum dimension from edge of alley is 5 ft.

Building stepbacks are in addition to building setbacks, and are established to create height and bulk transitions between buildings and public streets/ alleys/ parks, as well as between higher density uses and lower density uses. These transitions shall be controlled by building setback/ stepback requirements as shown in the illustrative sections included in this section of the document. Where stepbacks are required, 25% of the frontage is allowed without a stepback.

### Build-to line standard

Build-to-lines on both north and south sides of “C” Street are established along the required setback line (see illustrative sections). At least 60% of the ground floor building frontage at a minimum height of 15' shall be constructed along the established build-to-line. Porches located along this line are encouraged in this sub

area, and can be used to satisfy this requirement. In the event that porches encroach on setback as allowed in the setbacks section of this document; 100% of the ground floor of the building frontage shall be built to the build-to-line.

Other streets in this sub area are encouraged to follow this same build-to-line requirement.

### **Building Edge/ Facade Articulation**

Along build-to-line frontages, the intent in this sub area is to have the residential units aligned along a common setback in order to create a consistent and articulated urban edge of residential scale. No blank walls shall be allowed along the build-to-line frontages and facade articulation is required in the form of porches, entries, and other features to create visual interest.

### **Vehicular Driveway Access**

All vehicular access shall be taken via the required private alleys serving the sub area. Access streets and driveways are encouraged to reinforce the grid imposed by the street hierarchy of the Plan Area. This requirement may be waived through the Site Plan Review process if necessary.

### **Paseo - Mid Block Pedestrian Connection**

In order to promote pedestrian circulation and to provide meaningful massing breaks along the east west direction of the blocks in this sub area, mid block pedestrian connections called paseo are required. Such walkways shall have a minimum width dimension of 10 feet, shall be publicly accessible, and must align across alleys. This requirement may be modified through Site plan Review.

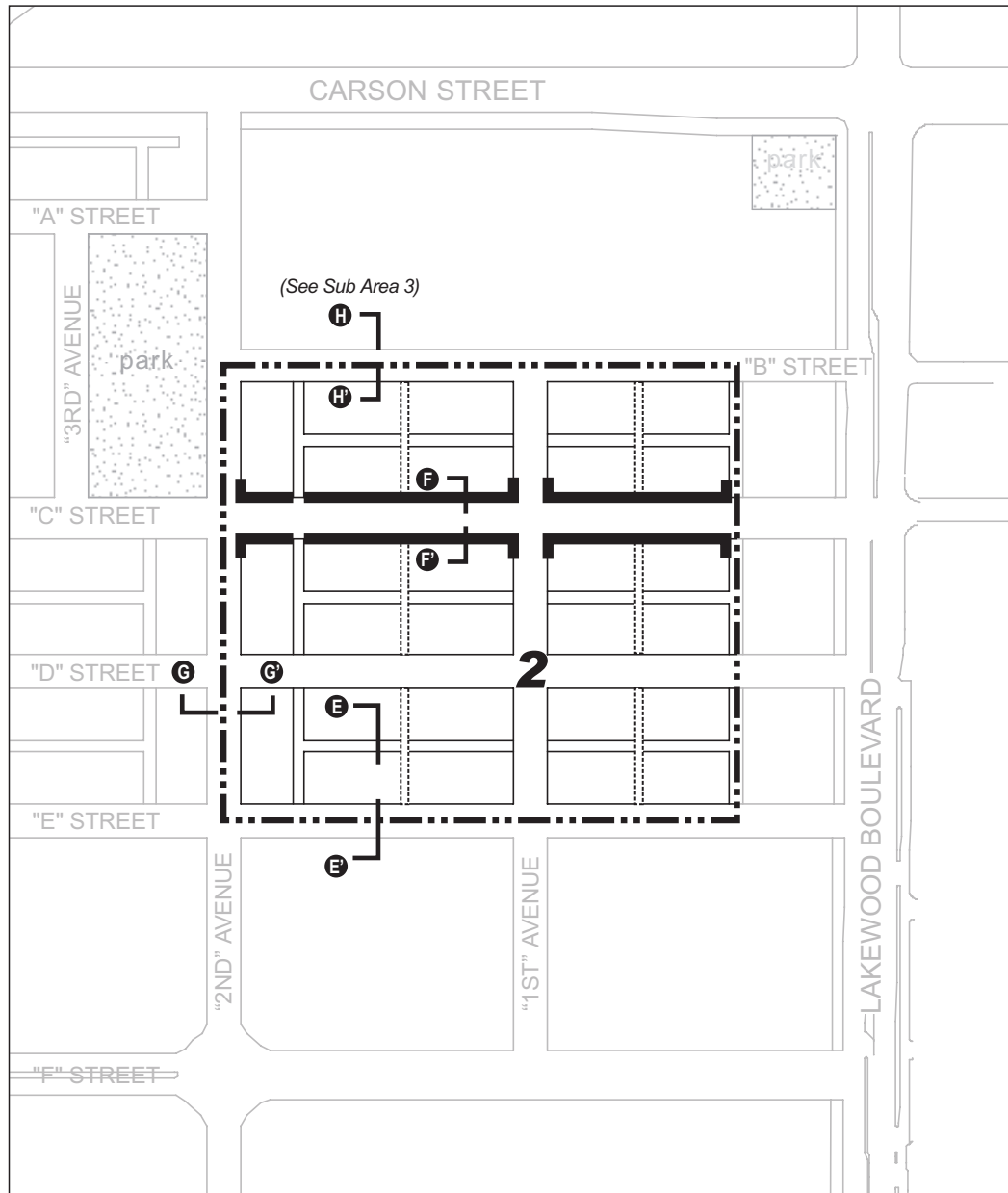
### **On-Grade Parking Garages**

#### *Location*

On-grade garages must be located at the rear of the lots and shall be accessible only through the private alley system serving this sub area. Parking in front yard setbacks is prohibited.

#### *Architectural treatment*

Garages must be designed with the same care and attention as the buildings they serve. They should be compatible in architectural treatment and detail.



Secondary 'Build-To' Lines



'Paseo' : Mid-Block  
Pedestrian Connection

Figure 16 : Sub Area 2

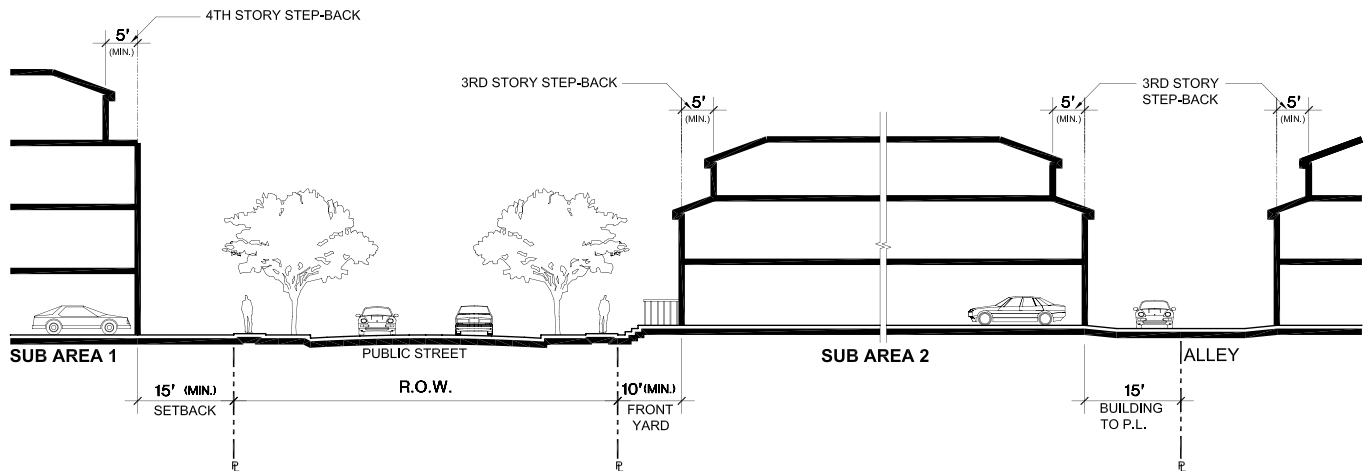
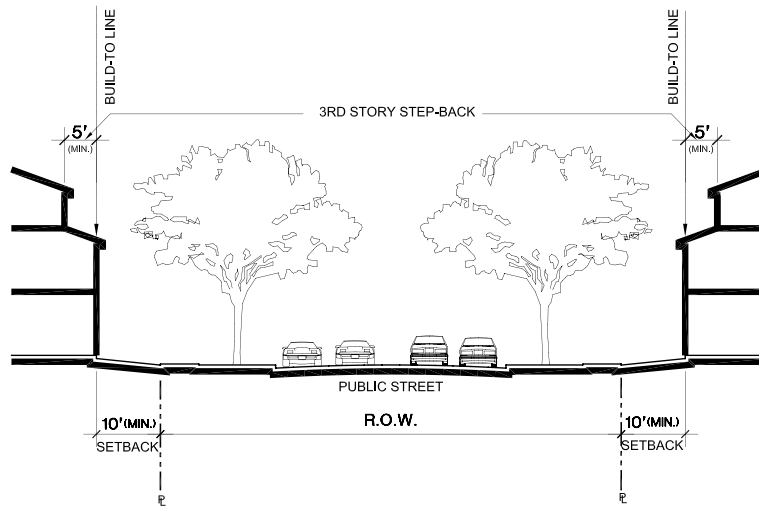


Figure 17 : Section E-E'



\*NOTE: 10' PREFERRED SETBACK TO ESTABLISH AN ARTICULATED STREET EDGE

Figure 18 : Section F-F'

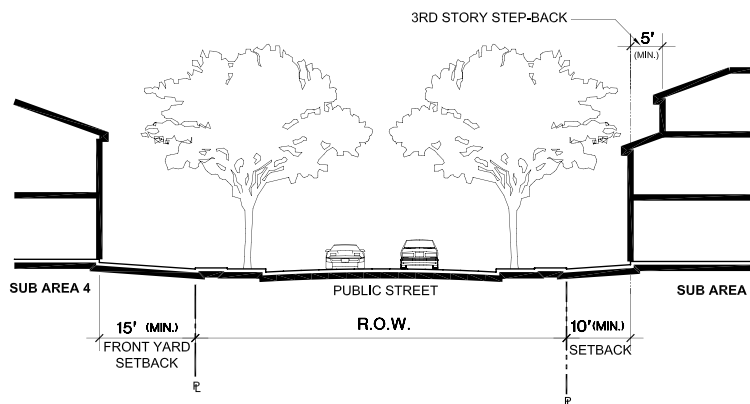


Figure 19 : Section G-G'

### Sub Area 3:

This sub area shall consist of moderate density, multifamily residential uses. It is intended to provide a moderate density use to be compatible with the existing older and lower density developments to the north. Along the Carson Street edge, this sub area shall provide a setback for the implementation of a bikeway connection.

#### Building Stepbacks

**Table 8 : Setbacks for Residential Sub Area 3**

Minimum Street Setback	Minimum Carson Street Setback	Minimum Bldg. to Bldg. Setback
15' *	10' **	8'

\* *Stoops and Porches may encroach 5 ft. into the Street Setback. Porches shall be as defined in the LBMC.*

\*\* *Measured from wall at the property line.*

Building stepbacks are in addition to building setbacks, and are established to create height and bulk transitions between buildings and public streets/ alleys/ parks, as well as between higher density uses and lower density uses. These transitions shall be controlled by building setback/ stepback requirements as shown in the illustrative sections included in this section of the document. Particular attention shall be placed in the area fronting the park on 2<sup>nd</sup> Avenue. Where stepbacks are required, 25% of the frontage is allowed without a stepback.

#### Bike Path Setbacks

A 30 foot parkway shall be provided immediately behind the street curb to accommodate a pedestrian sidewalk, Class I bike path, and associated landscape along the Carson Street frontage. It is envisioned that such residential uses will not front on Carson Street, and a minimum 10 feet rear setback (in addition to bike path parkway) is required for the residential units immediately adjacent to the bike path.

#### Facade Articulation

No continuous building wall shall extend more than 60 feet in width without a facade articulation element. Blank walls are discouraged, and the maximum width of a blank wall without articulation or relief of at least 6 inches in depth shall be 25

feet. Facade articulation shall consist of elements such as expressed structural bays, pilasters, moldings, recessed wall panels, or display features to create visual interest.

### **Vehicular Driveway Access**

All access streets and driveways that are built as part of this sub area shall reinforce the grid imposed by the street hierarchy of the Plan Area. Vehicular driveway access is prohibited from 2<sup>nd</sup> Avenue, and all vehicular access shall be taken via alleys or secondary neighborhood streets that will not disrupt pedestrian circulation. These requirements may be modified through the Site Plan Review process.

### **On-Grade Parking Garages**

#### *Location*

On-grade garages must be screened on street sides by residential units, the building entrance lobby or other portions of the building. The screening requirement may be waived through the Site Plan Review process only if the garage wall is deemed to be architecturally integrated into the design of the overall building, and is located along a secondary street.

#### *Architectural treatment*

Parking structures must be designed with the same care and attention as the buildings they serve. They shall be compatible in architectural treatment and detail.

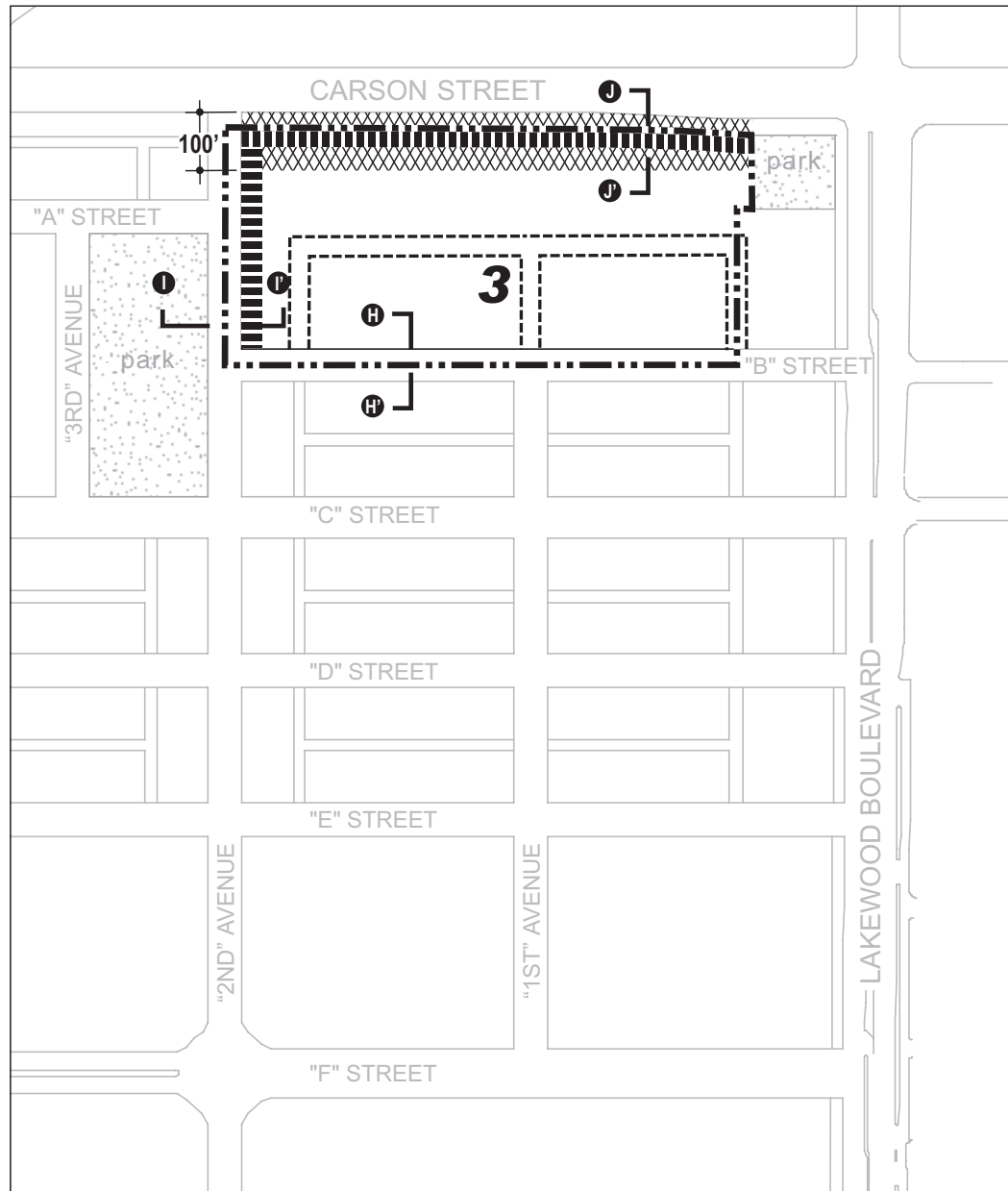
#### *Screening*

Ramps, cars, and sources of artificial lighting in parking structures shall be minimally visible from public streets, public parks, and residential uses.

### **Electrical Substation**

An electrical substation may be developed near the south-west corner of Lakewood Boulevard and Carson Street. Southern California Edison has provided general information on the size of the facility. The substation shall have a maximum footprint of approximately 230-feet by 305-feet, and power lines connecting to the substation shall be routed through new underground substructures.

The substation shall have a low profile structure (equipment will be approximately 12 feet in height) with underground feed lines, with an 8-foot perimeter masonry screen wall located at the building setback line, and associated perimeter landscaping between the right-of-way and the wall consisting of trees, shrubs, and ground cover.



35-foot height limitation

*This height zone runs from the curb at Carson Street to a line 100 ft. south of the curb, and from Lakewood Blvd. at the east to 2nd Street along the west.*



5' Minimum 3rd. Story 'Step-Back'  
(Refer to Sections I-I' & J-J')



Preferred Location of  
Access Streets

Figure 20 : Sub Area 3



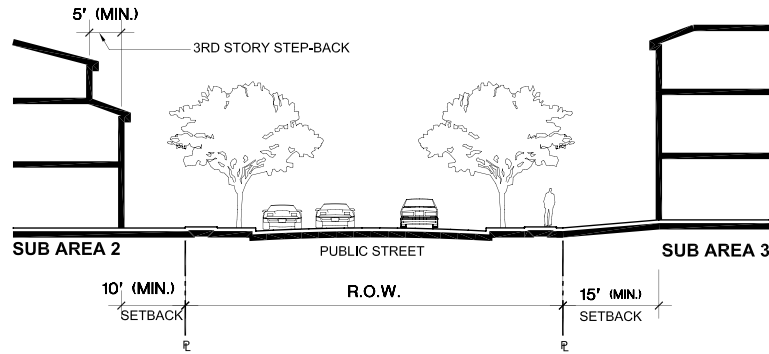


Figure 21 : Section H-H'

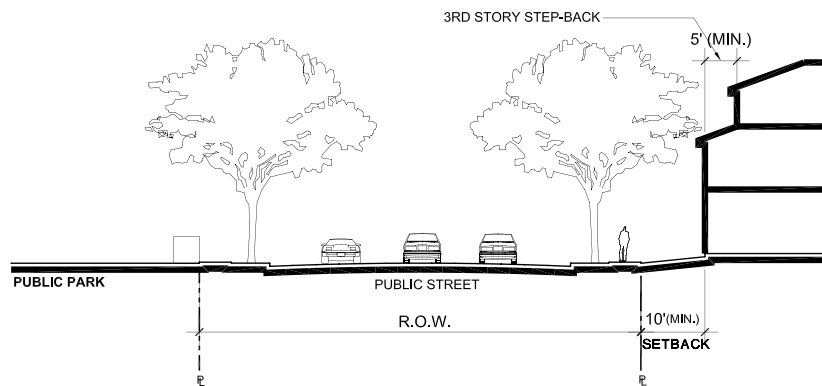


Figure 22 : Section I-I'

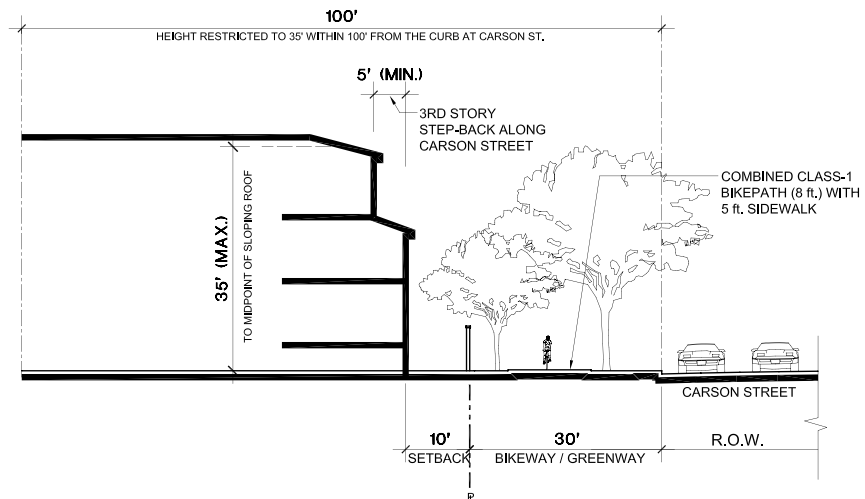


Figure 23 : Section J-J'

## Sub Area 4:

This sub area shall be a single-family residential district with small to moderate sized lots, and will be based on the character of traditional Long Beach neighborhoods, including the use of alleys for garage access. At the heart of this sub area, a public park will be located as a focal point of the neighborhood.

### Setbacks

**Table 9 : Setbacks for Residential Sub Area 4**

Minimum Lot Area	Minimum Street Setback	Minimum Porch Setback	Minimum Side Setback	Minimum Alley Setback
3500	Front - 15' * Side - 10' **	10' ***	4'	14' ****
4500	Front - 15' * Side - 10' **	10' ***	4'	14' ****

\* On any given block, 33% of the houses may have a habitable single story projection with a 10 ft. setback and a maximum width of 13 ft.  
50% of these houses may have a habitable second story projection of the same dimension.

\*\* Side setback to a privacy wall or fence is 5 ft. from the property line.  
On "Reverse Corner Lots" the setback to a privacy wall or fence is 10 ft.

\*\*\* 50% of the houses must have covered front porches with a minimum depth of 5 ft. and an area of at least 60 square feet.  
At the discretion of the Director of Building and Planning during site plan review, courtyard alternatives to front porches within the front setback may be allowed as part of the required usable open space.

\*\*\*\* Measured from property line at centerline of alley.

**Bike Path Setback**

A 30 feet parkway shall be provided immediately behind the street curb to accommodate a pedestrian sidewalk, Class I bike path, and associated landscape along the Carson Street frontage. Residential uses immediately adjacent to the bike path shall provide an additional 26' setback to incorporate access alleys.

**Building Edge/ Massing**

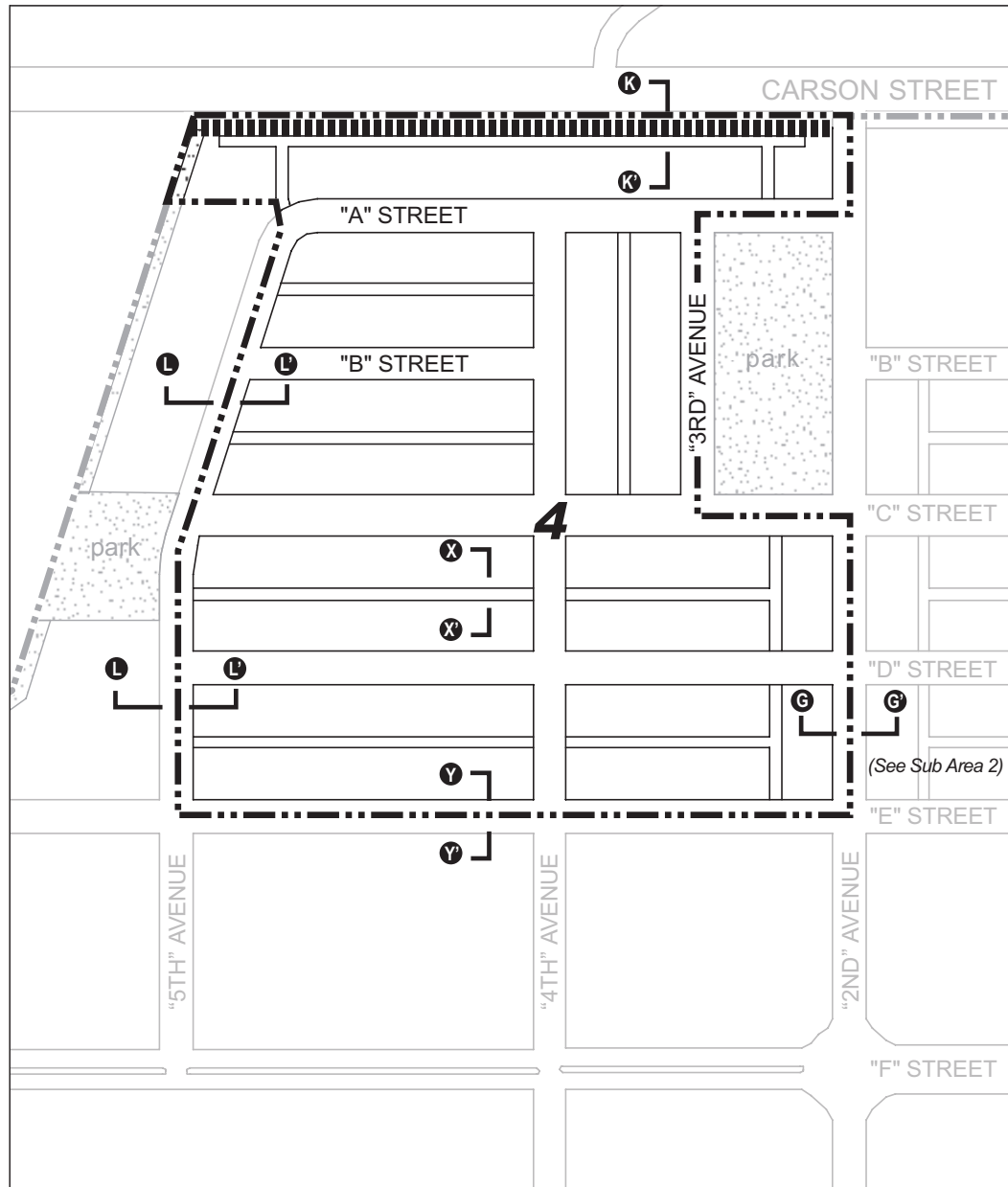
The intent for this sub area is to emulate historic Long Beach neighborhoods with consistent landscaped setbacks fronting the streets. Porches, stoops, and building entries shall be located along the street front to establish a consistent character, and pedestrian scale throughout the sub area. Building stepbacks and roofs shall be used to further articulate the buildings, and to enhance the pedestrian experience.

**Vehicular Driveway Access**

All vehicular access shall be taken via the required private alleys serving the sub area. Access streets and driveways are encouraged to reinforce the grid imposed by the street hierarchy of the Plan Area.

**On-Grade Parking Garages***Location*

On-grade garages must be located at the rear of the lots and shall be accessible only through the private alley system serving this sub area. Parking in front yard setbacks is prohibited.



 26' Minimum Building Setback  
 along Carson Street  
 (Refer to Section K-K')

**Note: There shall be a minimum of 100 lots of at least 4,500 s.f. in Sub Area 4**

Figure 24 : Sub Area 4

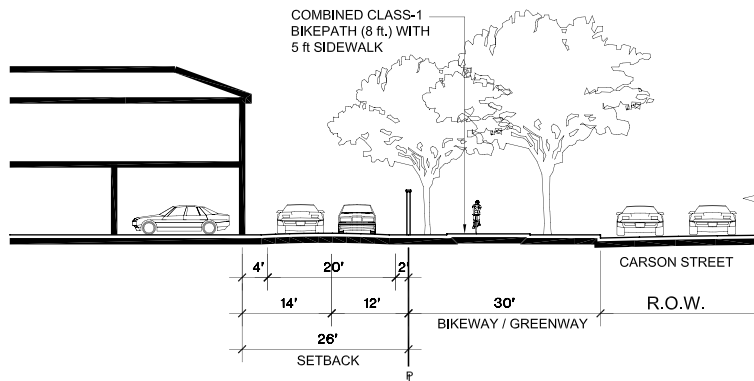


Figure 25 : Section K-K'

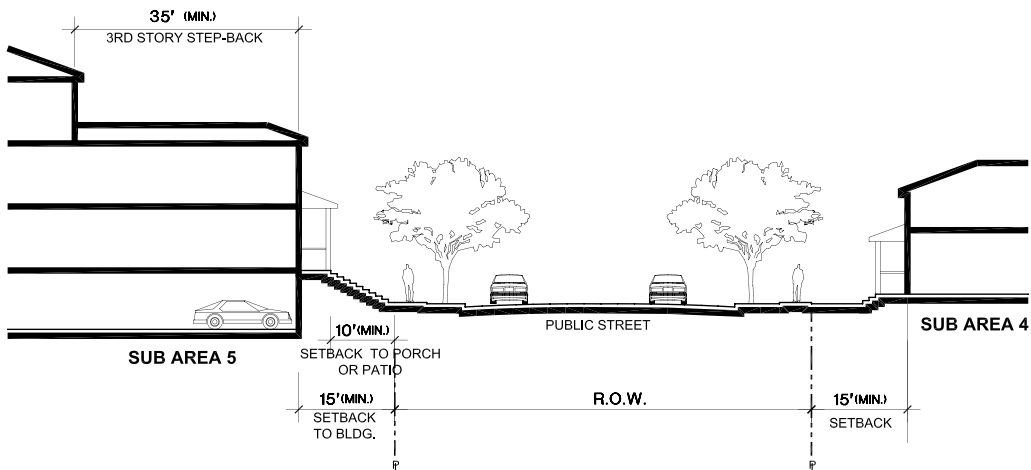


Figure 26 : Section L-L'

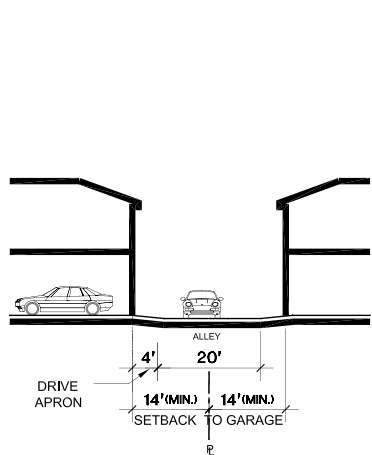


Figure 27 : Section X-X'

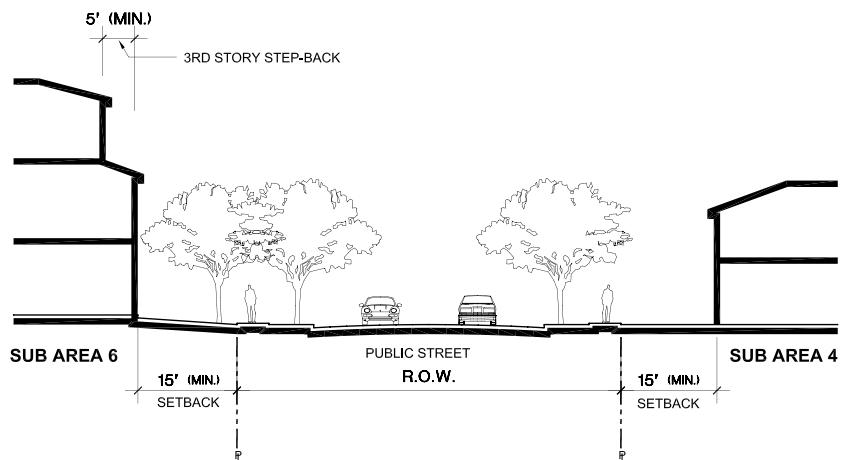


Figure 28 : Section Y-Y'

## Sub Area 5:

This sub area shall consist of moderate density, multifamily residential uses. This sub-area will serve as the project's edge to the existing Lakewood Country Club golf course, and it shall be designed as a porous urban edge that takes advantage of the golf course views and adjacency. A setback along the existing golf course edge shall contain a Class I bike path. The location of view corridors to the golf course and the design of a small public park shall be incorporated into this sub area.

### Setbacks and Building Stepbacks

**Table 10 : Setbacks for Residential Sub Area 5**

Minimum Street Setback	Minimum Rear Setback	Minimum Side Setback	Minimum Bldg. to Bldg. Setback
15' *	10' **	10' ***	8'

\* Stoops and Porches may encroach 5 ft. into the Street Setback.  
Porches shall be as defined in the LBMC.  
Setback to 3rd story is 50 ft.

\*\* Measured from the bike path easement. Balcony projections are not allowed within the setback.

\*\*\* Side yard setback at the northernmost building near 'A' Street is 58 ft. and 70 ft. to a 3rd story.

Building stepbacks are in addition to building setbacks, and are established to create height and bulk transitions between buildings and public streets/ alleys/ parks, as well as between higher density uses and lower density uses. These transitions shall be controlled by building setback/ stepback requirements as shown in the illustrative sections included in this section of the document. Where stepbacks are required, 25% of the frontage is allowed without a stepback.

### Bike Path Setback

A setback of 20 feet in width shall be provided along the existing golf course frontage to incorporate the required Class I bike path for the project. Residential uses immediately adjacent to the bike path shall provide an additional setback of 10 feet. Patios and balconies fronting this edge are encouraged, but shall not project over the required setbacks.

**Building Edge/ Facade Articulation**

No continuous building wall shall extend more than 60 feet in width without a facade articulation element. Blank walls are discouraged, and the maximum width of a blank wall without articulation or relief of at least 6 inches in depth shall be 25 feet. Facade articulation shall consist of elements such as expressed structural bays, pilasters, moldings, recessed wall panels, or display features to create visual interest.

**Vehicular Driveway Access**

All vehicular access shall be taken via secondary neighborhood streets that will not disrupt pedestrian circulation. Access streets and driveways are encouraged to reinforce the grid imposed by the street hierarchy of the Plan Area. This requirement may be modified through the Site Plan Review process.

**On-Grade Parking Garages***Location*

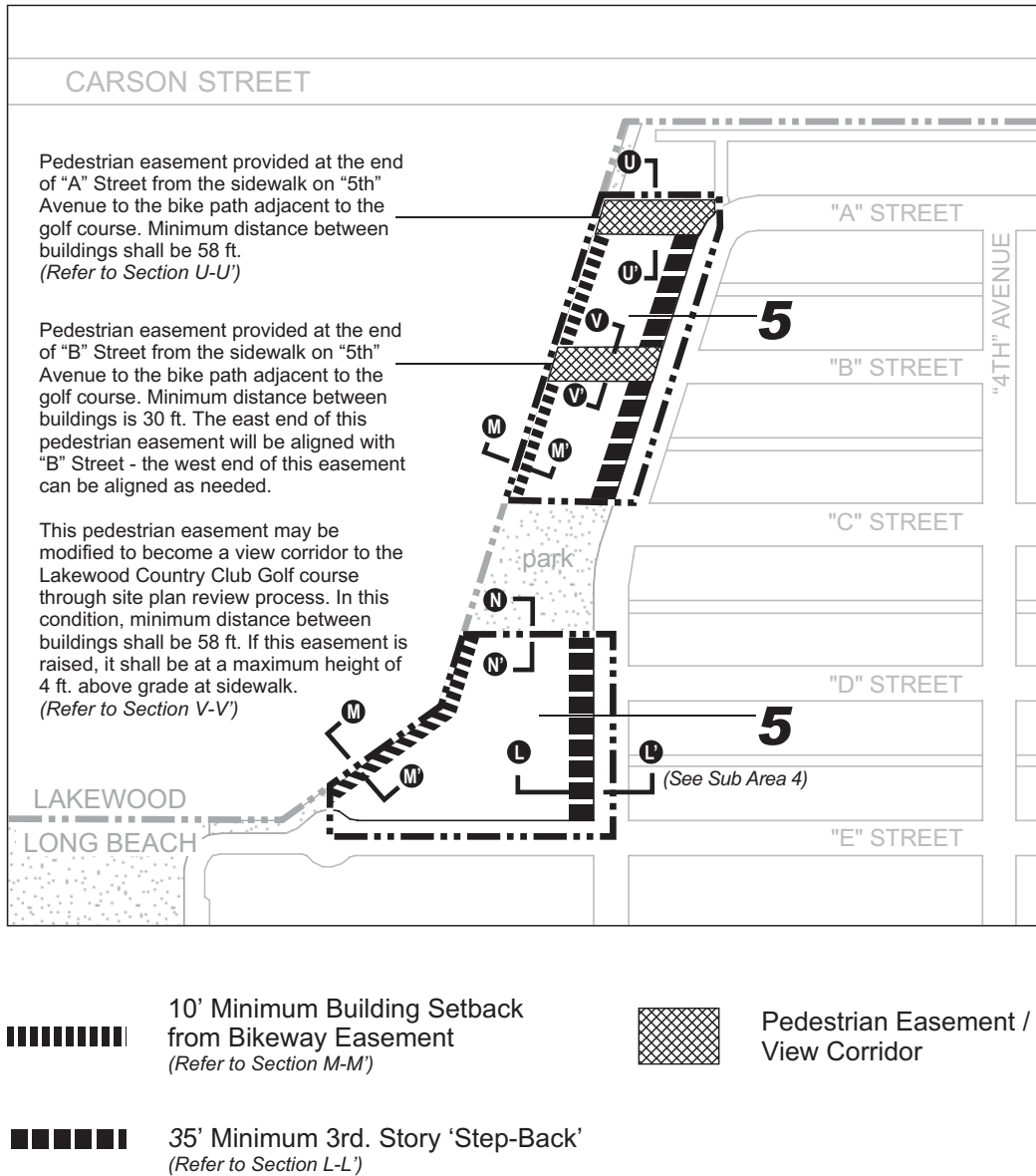
On-grade garages must be screened on street sides by residential units, the building entrance lobby or other portions of the building. The screening requirement may be waived through the Site Plan Review process only if the garage wall is deemed to be architecturally integrated into the design of the overall building, and is located along a secondary street.

*Architectural treatment*

Parking structures must be designed with the same care and attention as the buildings they serve. They should be compatible in architectural treatment and detail.

*Screening*

Ramps, cars, and sources of artificial lighting in parking structures should be minimally visible from public streets, public parks, and residential uses.



**Figure 29 : Sub Area 5**



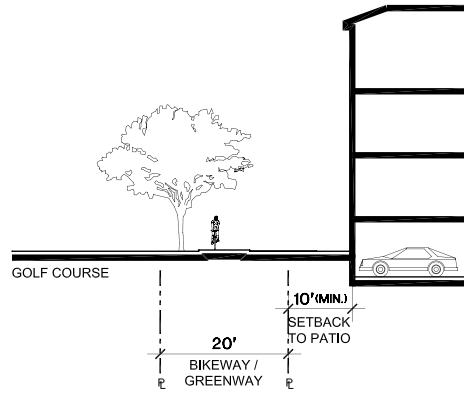


Figure 30 : Section M-M'

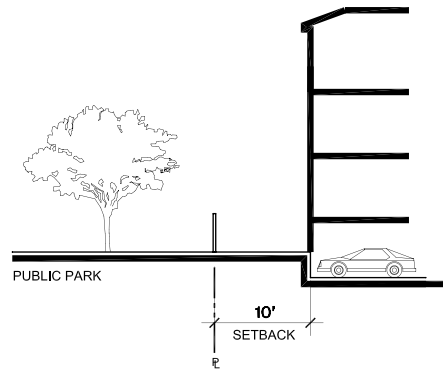


Figure 31 : Section N-N'

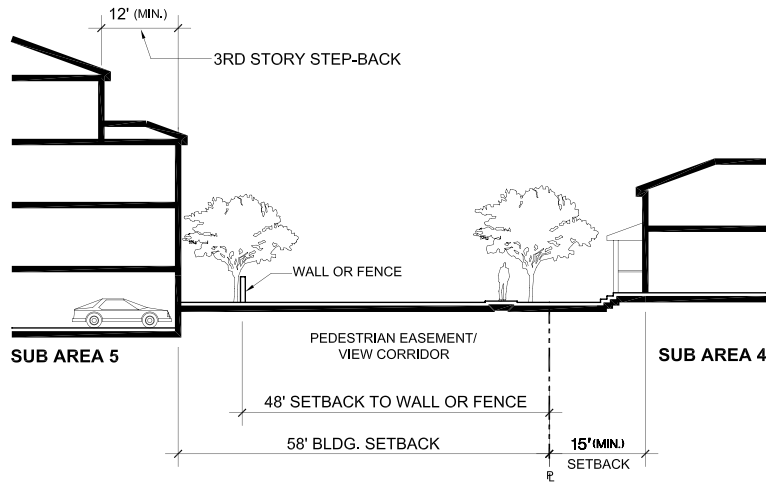


Figure 32 : Section U-U'

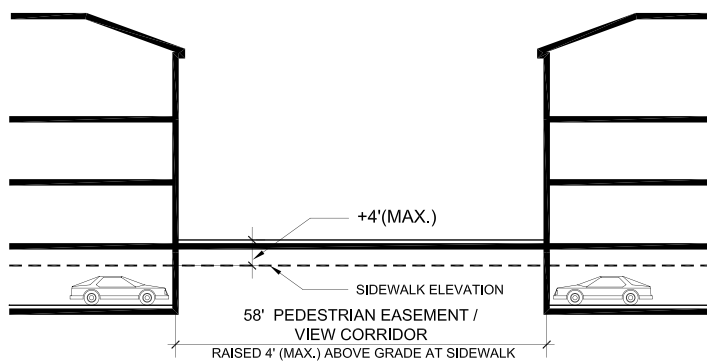


Figure 33 : Section V-V' - Option 1

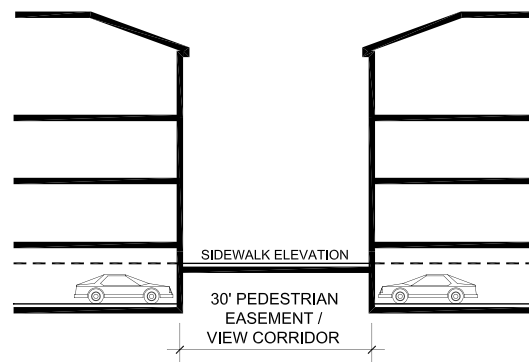


Figure 34 : Section V-V' - Option 2

## Sub Area 6:

This sub area shall consist of moderate density, multifamily residential uses. Located along “F” Street, it will serve as a continuation of the urban edge established in the mixed use district in Sub Area 1, and shall act as a buffer to the lower density residential uses to its north. A major public park will become the anchor to the west of the sub area, and shall be incorporated in the design of the sub area.

### Setbacks

**Table 11 : Setbacks for Residential Sub Area 6**

Minimum Street Setback	Minimum “F” Street Setback	Minimum Bldg. to Bldg. Setback
15' *	18'	8'

\*        *Stoops and Porches may encroach 5 ft. into the Street Setback.  
Porches shall be as defined in the LBMC.*

A continuous landscaped setback 18’ in width shall be provided along “F” Street as a way to buffer the residential units fronting the street, and as a unifying design element for both sides of the Street. If parking lots are located along this frontage, they shall maintain the required landscaped setback. The landscape design of such setbacks shall be coordinated with those in Sub Area 7 and 8A.

### Building Stepbacks

Building stepbacks are in addition to building setbacks, and are established to create height and bulk transitions between buildings and public streets/ alleys/ parks, as well as between higher density uses and lower density uses. These transitions shall be controlled by building setback/ stepback requirements as shown in the illustrative sections included in this section of the document.

**Facade Articulation**

No continuous building wall shall extend more than 60 feet in width without a facade articulation element. Blank walls are discouraged, and the maximum width of a blank wall without articulation or relief of at least 6 inches in depth shall be 25 feet. Facade articulation shall consist of elements such as expressed structural bays, pilasters, moldings, recessed wall panels, or display features to create visual interest.

**Vehicular Driveway Access**

All vehicular access shall be taken via alleys and secondary neighborhood streets that will not disrupt pedestrian circulation. Access streets and driveways are encouraged to reinforce the grid imposed by the street hierarchy of the Plan Area. This requirement may be modified through the Site Plan Review process.

**On-Grade Parking Garages***Location*

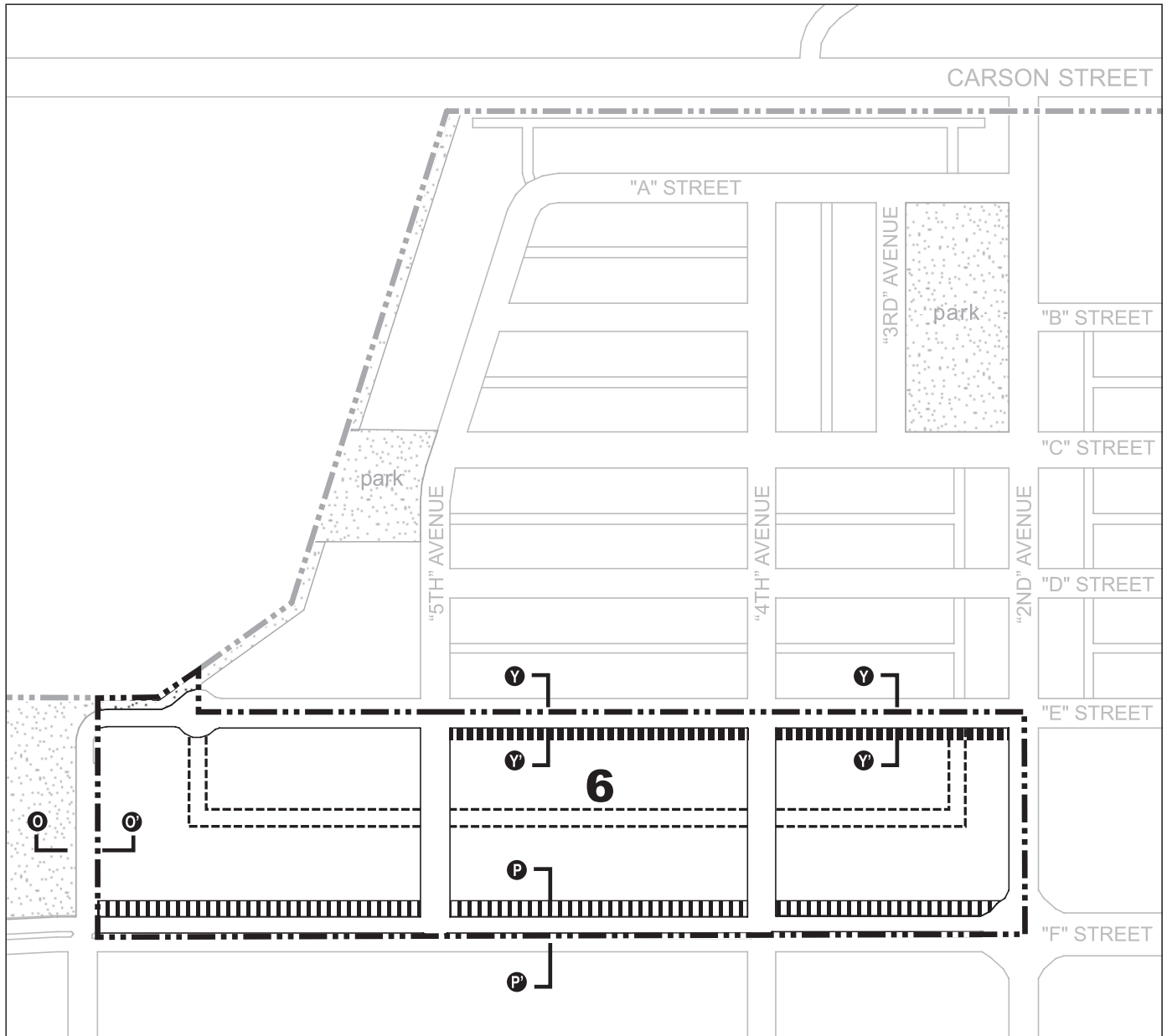
On-grade garages must be screened on street sides by residential units, the building entrance lobby or other portions of the building. The screening requirement may be waived through the Site Plan Review process only if the garage wall is deemed to be architecturally integrated into the design of the overall building, and is located along a secondary street.

*Architectural treatment*

Parking structures must be designed with the same care and attention as the buildings they serve. They should be compatible in architectural treatment and detail.

*Screening*

Ramps, cars, and sources of artificial lighting in parking structures should be minimally visible from public streets, public parks, and residential uses.



18' Minimum Building Setback  
along F Street



Preferred Location of  
Access Streets



5-foot step-back at third story along street-  
frontage and alleys throughout sub area 2  
(Row-houses)

Figure 35 : Sub Area 6

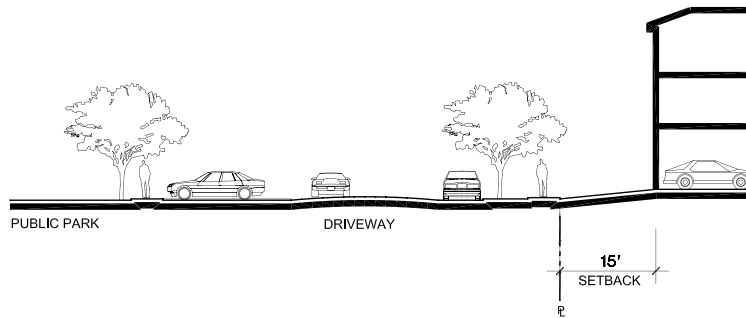


Figure 36 : Section 0-0'

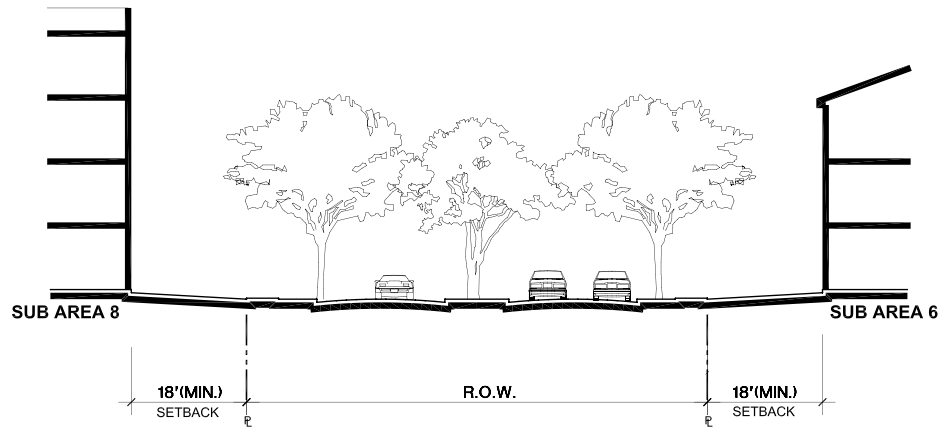


Figure 37 : Section P-P'

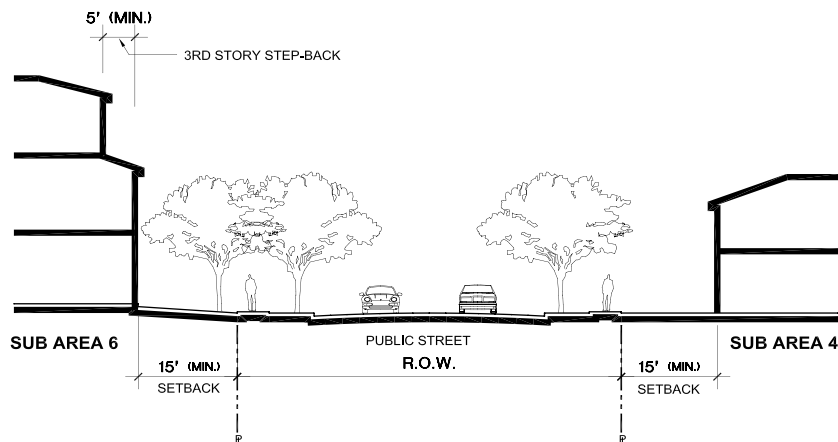


Figure 38: Section Y-Y'

